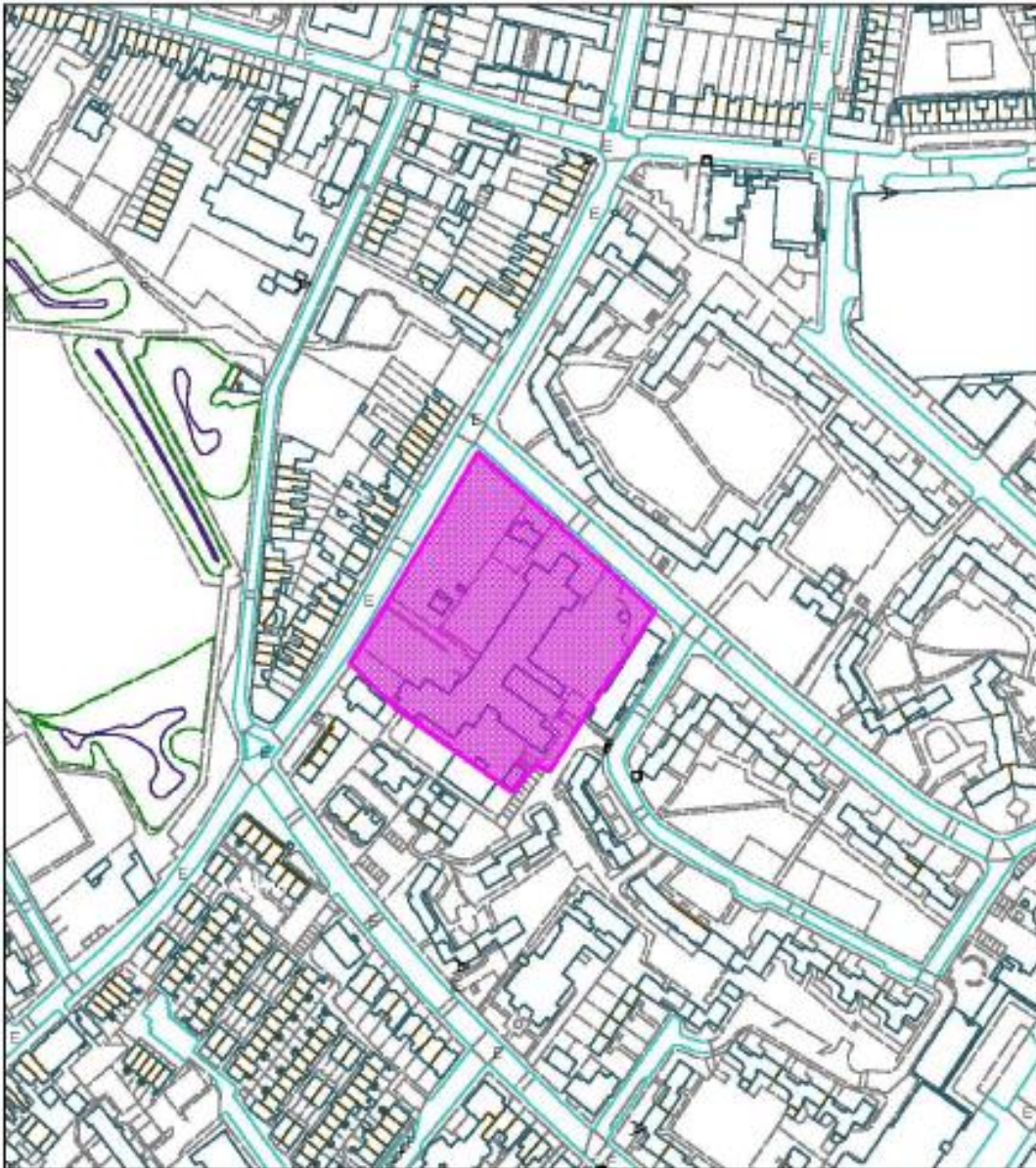


Case Number:	17/01473/FUL
Application Address:	Allen Edwards Primary School



Scale: 0 25 50 100 Meters

Reproduced from the Ordnance Survey map with permission of the Controller of Her Majesty's Stationery Office © Crown Copyright 2012.

Unauthorised reproduction infringes Crown Copyright and may lead to prosecution or civil proceedings.

L.B. Lambeth LA 100019338

Address:	Allen Edwards Primary School, Studley Road, London, SW4 6RP	
Application Number: 17/01473/FUL	Case Officer: Dale Jones	
Ward: Stockwell	Date Validated: 07.04.2017	
Proposal: Demolition of the existing main school building and the erection of part 2/part 3 storey primary school building and associated landscaping.		
Drawing Numbers:		
<p>001 Rev.1 (Tree Constraints Plan); 08261-A-L-(00)-0200 P1; 08261-A-L-(00)-0201 P1; 08261-A-L-(00)-0202 P1; 08261-A-L-(00)-0203 P1; 08261-A-L-(00)-0204 P1; 08261-A-L-(00)-0205 P1; 08261-A-L-(00)-0206 P1; 08261-A-L-(00)-0208 P1; 08261-A-L-(00)-0210 P3; 08261-A-L-(00)-0212 P3; 08261-A-L-(00)-0214 P3; 08261-A-L-(00)-0216 P1; 08261-A-L-(00)-0232 P2; 08261-A-L-(00)-0233 P2; 08261-A-L-(00)-0234 P2; 08261-A-L-(00)-0235 P2; 08261-A-L-(00)-0240 P2; 08261-A-L-(00)-0240 P1; LN0012-AG-ZZ-01-DR-M-1101 Rev P01; LN0012-AG-ZZ-01-DR-M-1102 Rev P01; LN0012-AG-ZZ-01-DR-M-1103 Rev P01; LN0012-AG-ZZ-01-DR-M-1104 Rev P01; LN0012-AG-ZZ-01-DR-M-1105 Rev P01; LN0012-AG-ZZ-01-DR-M-1106 Rev P01; LN0012-AG-ZZ-00-DR-E-2301 Rev P01; LN0012-AG-ZZ-00-DR-E-2302; 1278_GA_100 Rev S; 1278_GA_100 Rev S; 1278_GA_103 Rev C (Fencing Plan); 1278_PP_300 (Planting Plan); Fire Plan (Phase 1, 2 and 3); Logistics Plan (Phase 1, 2 and 3) and Cover Letter dated 24th March 2017.</p>		
Documents:		
<p>Tree Survey and Tree Constraints Plan (31.03.2017); Air Quality Assessment (February 2017); Transport Statement (March 2017); Building Regulations Part L and EPC Report (06.01.2017); Daylight and Sunlight Report (8th March 2017); Preliminary Ecological Appraisal (March 2017); GLA Energy Assessment (12.02.2017); Environmental Noise Assessment (17.03.2017); Flood Risk Assessment (17.03.2017); Sustainable Drainage Statement (24.03.2017); Phase 2 Geo-Environmental Assessment (May 2017); Remediation Strategy Report (May 2017); Arboricultural Implication Assessment (16.05.2017); Arboricultural Method Statement (16.05.2017); BREAEAM Pre-Assessment Scoring Sheets 1-53; Design and Access Statement Rev P3; Letter from the Education and Skills Funding Agency (dated 12th June 2017); Heritage Assessment (June 2017); Planning Statement (March 2017); BREEAM Tracker (July 2017); Construction Method Statement (3rd April 2017); Biodiversity Note (29th May 2017); Community Use Lettings Policy (20th June 2017; Adaptive Comfort and Indoor Air Quality Assessment (20th June 2017).</p>		
RECOMMENDATION:		
<p>(i) Resolve to grant conditional planning permission subject to completion of an agreement under Section 106 of the Town and Country Planning Act 1990 for the obligations listed in this report.</p>		

<p>(ii) Agree to delegate authority to the Assistant Director of Planning, Transport and Development to finalise the recommended conditions and section 106 legal agreement as set out in this report.</p> <p>(iii) In the event that the committee resolves to refuse planning permission and there is a subsequent appeal, delegated authority is given to officers, having regard to the heads of terms set out in the report, to negotiate and complete a document containing obligations pursuant to Section 106 of the Town and Country Planning Act 1990 in order to meet the requirement of the Planning Inspector.</p>	
<p>Applicant: Allen Edwards Primary School, Interserve Plc, Dolphin Bridge House Rockingham Road, Uxbridge, UB8 2XL</p>	<p>Agent: Mrs Laura Meyer, GL Hearn, Vantage Point, Hardman Street, Spinningfields, Manchester, M3 3HF</p>

SITE DESIGNATIONS

Relevant site designations	
SBNCI - Sites of Borough Nature Conservation Importance	Oasis Nature Garden SNCI

LAND USE DETAILS

	Use Class	Use Description	Floor space (GEA)
Existing	D1	Non-residential institutions as a primary school	3,069.3
Proposed	D1	Non-residential institutions as a primary school	2,465.0 (-332.8sqm)

EMPLOYMENT DETAILS

	Full-time	Part-time
Existing employees	46	-
Proposed employees	46	-

PARKING DETAILS

	Car Parking Spaces (general)	Car Parking Spaces (Disabled)	Bicycle Spaces	Motorbike spaces
Existing	23	0	12	0

Proposed	21	2	46	0
-----------------	----	---	----	---

EXECUTIVE SUMMARY

The Priority School Building Programme (PSBP) is a privately financed public private partnership (PPP) programme that addresses the needs of the schools that are most in need of urgent repair. Through the £4.4 billion programme, 260 schools will be rebuilt or have their condition needs met by the Education Funding Agency (EFA). The programme only seeks to address the condition need of existing schools and does not seek to deliver school expansion.

Alongside 260 other schools, Allen Edwards Primary School successfully applied for the PSBP Programme based on the existing schools existing condition need.

The proposed development seeks the demolition of the existing main school building and caretakers lodge and the erection of a new part 2, part 3 storey building towards the north of the site, adjacent to Studley Road. The existing music block and nursery are to be retained as existing.

The proposed reconfiguration of the school building would allow for the provision of a larger playground towards the south of the site, allowing more open space for school children for learning and play without compromising pupil or staff numbers.

The new school building has been designed to respond to and be respectful of the height and massing of neighbouring buildings, it is appropriate to the area and is not considered that it would harm the setting of the adjacent Heritage Assets. Furthermore, the siting, design and scale have been designed to safeguard neighbouring amenity.

A transport assessment demonstrates that the new school would not have a negative impact on the local transport network. The proposed development would bring positive benefits to the local community in terms of improved educational facilities in the area; it would also enliven the site through a well thought-out, high quality designed building with enhanced landscaping.

Overall, the scheme is considered appropriate and acceptable, and is recommended for approval subject to the recommendations made on the cover sheet to this report.

OFFICER'S REPORT

Reason for referral to PAC: In accordance with item (1) (ii) of the Committee's terms of reference, it is necessary to report this application to committee in any event as it relates to the provision of a building or buildings where the floor space to be created including changes of use, by the development is 1,000sqm or more.

1 SITE AND SURROUNDINGS

- 1.1 The site currently comprises Allen Edwards Primary School, a 2FE mixed sex primary school which has 420 pupils, 40 nursery pupils and 46 staff members. The application site is located on a roughly rectangular piece of land that covers an area of approximately 1000sqm (1.0 hectare). The site is located to the south of the junction of Studley Road and Larkhall Lane. The site is located to the north of Jeffery's Road and the west of Paradise Road. The site is adjacent to the Oasis Nature Garden and is bound on all other sides by residential uses.



Figure 1. The existing school site and context

- 1.2 Originally built in 1956, the main school building is located in the centre of the site and comprises a part single, part two storey red brick building. A dilapidated and vacant single storey caretakers unit is located separately towards the north west of the site and a music block and nursery are located separately towards the south. The old caretaker's accommodation (which is ancillary to the school within D1 class use) has a separate entrance point on Studley Road and is surrounded by separate railings. Access to the nursery is gained via Paradise Road.
- 1.3 Pedestrian access to the school building is predominately via Paradise Road, with a secondary entrance point located on Studley Road. Vehicular access to the site is gained from Larkhall Lane; where there is a small staff car park providing 23 spaces.
- 1.4 No part of the site is situated within a Conservation Area and there are no listed buildings on the site. The Oasis Nature Garden, located to the north-west of the site, is designated as a Site of Borough Nature Conservation Importance. Additionally, the site

sits adjacent to the Larkhall Conservation Area, which runs on the northern side of Larkhall Lane. Listed buildings within the surrounding area include the Grade II listed 76 and 78 Larkhall Lane, and the locally listed 88-94 Larkhall Lane.

- 1.5 The site is located within a Local View Protected Vista for Brixton Historic Towers and Brixton Panoramic, located towards the North West.



Figure.2 indicating the existing part one, part two storey school building

The Surrounding Area

- 1.5 With regard to the site context, the area immediately surrounding the site is predominantly residential, with a block of six storey flats located adjacent to the site on Paradise Road and a block of four storey flats adjacent on Studley Road. Semi-detached, detached and terraced houses can be found opposite the site on Larkhall Lane.
- 1.6 Towards the south, the site borders a four storey block of flats off Jeffrey's Road and Jeffrey's Walk. Larkhall Park is located approximately 70m east of the school and an Oasis Nature Garden is located on the north western corner of the site.
- 1.7 The site is located approximately 0.3 miles west of Stockwell Underground Station, which operates on Northern and Victoria Lines. Clapham. The site is well connected by road; being approximately 335m east of Wandsworth Road and 370m west of the A3, Clapham Road.

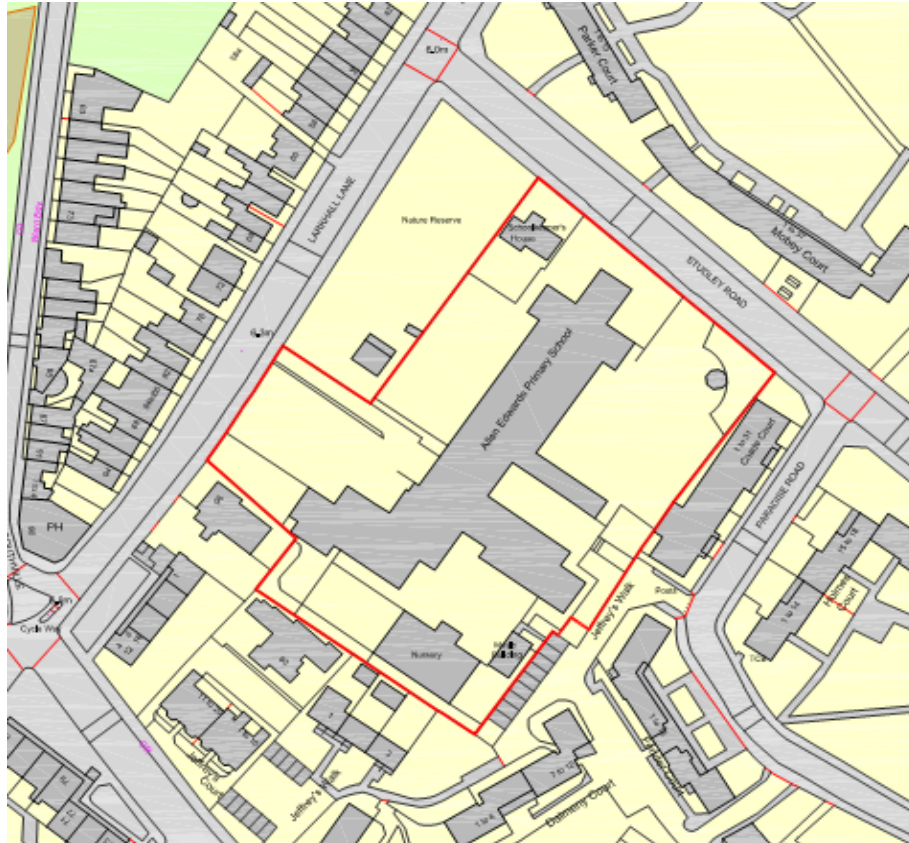


Figure 3. OS Map of the site and the surroundings

2 PROPOSAL

2.1 The application seeks full planning permission for the proposed demolition of the main school building and redevelopment to allow for construction of new part 2, part 3 storey primary school building and associated landscaping. The existing nursery and music block located towards the south of the site will be retained.

2.2 Key headlines of the proposed redevelopment scheme include:

- The proposed works will not lead to an increase in student intake or staff employment. The school currently has 46 members of staff and can accommodate a total of 420 pupils and 40 nursery children;
- The Council is funding a 15 place Speech, Language and Communication Needs (SLCN) unit at the school, which will encompass two flexible classroom spaces. These pupils form part of the overall 420 primary school places;
- Allen Edwards Primary School is a previously established Local Authority funded primary school. All buildings on site are considered to be of D1 use. The old caretakers unit, which is set to be demolished, is considered ancillary to the school and therefore also D1 in use;
- Buildings to be demolished include the part one, part two storey existing main school building and the caretakers lodge (a total of 2797.8sqm GIA). The new part two, part three storey school building would cover a reduced footprint of 1,101sqm resulting in a larger external play space of 8864sqm to the rear of the building;

- The school will be a mix of two and three storeys, which is in keeping with the height, mass and scale of neighbouring buildings;
- Increasing the height of the building will allow for the provision of an expanded outdoor space. In terms of landscaping, the large outdoor space will offer a diverse range of functions with an informal play zone to the east, formal play zone to the south and a growing zone (with greenhouses) to the west;
- The reconfiguration of the school building will allow for the provision of a larger playground towards the south of the site, allowing more open space for school children;
- Two new entrance points, one for visitors and one for reception pupils would be created on Studley Road. The main pedestrian pupil access would continue to be taken from Paradise Road, with access to the nursery obtained via a separate gate on Paradise Road;
- The loss of trees and replanted species together with improved landscaping scheme.
- Internally the building layout has been thoroughly considered to create a simple and legible layout, with all rooms accessed via a single central corridor, which is flanked on both ends by stair cores. The school and community benefit from having a large hall, small hall and studio that can be used for external groups at weekend and evenings through a secure entry that does not allow for access to the rest of the school.



Figure 4 indicating the existing condition of the school and playground

2.3 By way of context, the Education Funding Agency (EFA) have submitted a supporting letter which sets out the development rationale and states that: Allen Edwards Primary School was successful in its application to the Priority School Building Programme which was established purely to address condition need of school buildings. The programme does not seek to deliver school expansion. If additional pupil places are required these would be funded and delivered separately by the Local Authority. The Council has a duty under the Education Act 1996 to provide sufficient schools, in terms of number, character and equipment, to provide all pupils with the opportunity of appropriate education. Furthermore, it is also considered that

this scheme would not prejudice the future expansion of school places on this site if required.

3 RELEVANT PLANNING HISTORY

- 3.1 Planning application (Ref. 03/01602/FUL) for the proposed rebuilding of the nursery and playground including erection of covered play areas, covered walkways between nursery and main school and in front of 4 no. classrooms with associated alterations was approved on 18.09.2003.
- 3.2 Planning application (Ref. 13/05048/FUL) for the proposed change of use of existing caretaker's bungalow (Use Class C3) to conference/meeting rooms (Use Class B1) with associated internal alterations was withdrawn on 31.12.2013.

4 CONSULTATIONS

4.1 Statutory Consultees:

4.2 **Environment Agency:** No objection to the planning application as submitted, stating that they will not be providing extensive comments on flood risk grounds, as the site is situated within Flood Zone 1, which is defined as having a 'low probability' of river or sea flooding.

4.3 **London Fire Brigade:** Raise no objections to the proposals stating that:

From the information supplied it would appear there is ample opportunity to provide code compliant' Firefighting vehicular access via Studley Road, Paradise Road, Larkhall Lane (staff car park). Therefore, the London Fire Brigade would expect suitable firefighting facilities in line with those outlined in B5 of Approved Document B.

Further to this it is noted the works relate to the extension of or alteration to an existing school building, it is anticipated that Building Bulletin 100; Design for fire safety in schools (BB100) will be used to inform the design. This Authority fully supports the guidance given within BB100, and would recommend that the cost benefit analysis and risk assessment tools that accompany the document are completed in relation to the proposed works, giving specific consideration to the application of sprinklers and fire compartmentation. If these have already been completed, then for consistency we would request copies of them.

London Fire Brigade will have sight of the building regulation's consultation once submitted but would welcome a pre consultation pack and the opportunity to pass comment prior to works commencing.

4.4 **Lambeth Conservation and Design:** No objection to the revised scheme. Recommended conditions in relation to materials and construction details.

4.5 **Lambeth Transport:** No objections to the proposal from transport planning perspective subject to conditions and the applicant entering into a S106 agreement:

- Travel plan monitoring fee
- Further details of Deliveries and Servicing

The applicant should note that a full Travel Plan will be secured via condition, prior to occupation. In addition, the Travel Plan could include more focus on encouraging cycling, from the current low base, and also to take advantage of the Cycle Quiet way which runs along Larkhall Lane.

4.6 **Sustainability Consultant (Bio-Regional):** – The scheme is acceptable from a sustainability perspective as the pre-assessment shows a compliant score is targeted. However, this must be conditioned – that a Design Stage BREEAM assessment is undertaken and submitted to the Planning Authority for their approval prior to the commencement of above ground works showing how BREEAM Very Good and a minimum score of 63% will be achieved in practice, as per Lambeth Local Plan Policy EN4.

4.7 **Lambeth Arboricultural Officer:** The scheme is acceptable subject to conditions including Tree Protection Plan.

4.8 **Design out Crime Officer:** No objections subject to conditions to require the development to meet Secured by Design (New Schools 2014), and the adoption of these standards will help to reduce the opportunity for crime, creating a safer, more secure and sustainable environment.

4.9 **Lambeth Planning Policy:**

No objections were raised by the Council's Planning Policy team. The current primary schools building blocks to be demolished in total. Local Plan Policy S3 (a) states that proposals for new primary and secondary schools, or for the extension or expansion of existing schools, will be supported where they help to deliver the council's agreed strategy for provision of additional state funded school places in the borough. Submitted documents reveal that the proposed school will accommodate all of its current 420 pupils.

Part (d) of Policy S3 states that proposals for the shared use of schools for wider community use will be supported provided that the nature and scale of the proposal does not unacceptably harm the amenity of the area. In turn part (e) outlines that proposals should support sustainable travel, including the need for school travel plans and appropriate road danger reduction measures.

4.10 **Veolia Environmental Services:** Raised no objections stating that confirmation required that access to the bin storage area has a dropped kerb and also confirmation required regarding the pulling distance from bin store to refuse vehicle.

4.11 **Environmental Support Officer (Regulatory Services):** Raised no concerns subject to the use of planning conditions relating to (i) land contamination, (ii) construction phasing, (iii) noise control, (iv) air quality and (v) external lighting

4.12 **Lambeth Building Control:** We would not have any comments upon a demolition application under planning as it is a requirement under the Building Act that a demolition application is sent to us and this has to be agreed before demolition can commence.

4.13 **Lambeth Parks and open spaces** raise no objections stating that:

The direct risk to the adjacent Borough Site of Importance for Nature Conservation (Borough SINC), namely the Oasis nature Garden (SINC08, old code LaBII06), is relatively minimal and provided there is no intention to encroach onto or intrude into the SINC, which would result in new build or loss of natural space, then Parks and Open Spaces would be supportive of this application. A condition should be added to ensure that applicants implement all of the key recommendations in the Preliminary Ecological Appraisal.

4.14 **Adjoining owners/occupiers and amenity groups**

4.15 The Town and Country Planning (Development Procedure Order) 2015 requires the Local Planning Authority (LPA) to publicise applications for planning permission in the requisite manner.

4.16 Site notices have been erected on site by officers from displayed from 09 May 2017 and the application was advertised in the local paper (South London Press) on 14th April 2017.

4.17 A total of 170 letters were also posted to surrounding properties along Jeffrey’s Road, Paradise Road, Studley Road and Larkhall Lane.

4.18 A total of two objections have been received in response to the Council’s public consultation exercise. The comments are set out within the context of the table below:

Summary of objections	Response
Transport and Construction Phase issues	
Our property backs onto the school at the rear of the Nursery block. Our main concern is security during the build as the fence to our garden is hidden behind the school building –	The nursery block is being retained and is not altered as a result of the proposals therefore there will be no impact on the neighbour’s garden/fence.
Potential; volume of works traffic and general building noise if all these projects happen at the same time.	The draft Construction Management Plan details the volume of works traffic and building noise relating to this application. Construction Vehicles enter the site from Larkhall Road, not Jeffrey’s Road. Please refer to section 6 of the officer report below.

<p>What is the actual time scale for each phase of the project and who is overseeing the progress of this</p>	<p>The submitted phasing plan sets out the main programme works. The precise timings are not a planning issue. Again, the Construction Management Plan will be secured by way of planning conditions (Nos. 3 and 13)</p>
<p>What precautions are going to be taken to minimise dust and other pollution during the works?</p>	<p>The applicants have indicated that when demolition is carried out scaffolding will be sheeted with mono flex. Damping down is carried out throughout alongside regular wheel cleaning. Please see submitted Construction Management Plan for more information. Please refer to Paragraphs 6.94 – 6.95 and Conditions 3 and 13.</p>
<p>What are the penalties for the builders if they over run?</p>	<p>This is a non planning issue</p>
<p>Given the very good transportation links, why has so much space been assigned for car parking, the developers plan shows space for 33 cars, this seems excessive</p>	<p>The level of car parking will remain as existing (23 spaces, 2 disabled) at the request of the local highways authority. There will be no increase. Please refer to Paragraphs 6.79 – 6.80.</p>
<p>Accessibility and adaptability concerns</p>	
<p>Wheelchair accessible toilets - room width and depth specified, basin impedes access for a carer, space appears to be too small Hygiene room, no shower facility can be seen in the plan, no toilet either.</p> <p>Will the changing bench be height adjustable? Will a hoist be provided and will this be compatible with the slings the children are used to at home?</p>	<p>The internal plans have been developed in line with the requirements of the EFA and the school. The requirements for the hygiene room are a sink and a bed. A shower and toilet are not required.</p>
<p>Concerns relating to inaccuracies of the information including errors within the Design and Access statement relating to the classification of the neighbouring Coade Court.</p>	<p>The applicants have since advised that one of the diagrams did not mention that Coade Court dropped to Ground+3 storeys although this was mentioned elsewhere. Two lines of text have been added to the updated D&A to correct this. Officers have secured the updated D&A to reflect</p>

	this inconsistency, and it was not considered necessary to re-consult given that the scales were clear throughout the submitted plans and elsewhere within the D&A statement.
The plans have no annotated dimensions.	All drawings are to scale therefore the measurements can be determined easily by reading the drawings. The building is located in the correct position, relevant to all surroundings as per drawing validation requirements.

5 POLICIES

- 5.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires planning decisions to be made in accordance with the development plan unless material considerations indicate otherwise.
- 5.2 The National Planning Policy Framework was published in 2012. This document sets out the Government's planning policies for England including the presumption in favour of sustainable development and is a material consideration in the determination of all applications.
- 5.3 The development plan in Lambeth is the London Plan (2016) and the Lambeth Local Plan (September 2015).
- 5.4 The current planning application has been considered against all relevant national, regional and local planning policies as well as any relevant guidance. Set out below are those policies most relevant to the application, however, consideration is made against the development plan as a whole.
- 5.5 The London Plan (2016, MALP)

Relevant policies include:

Policy 1.1 - Delivering the Strategic Vision and Objectives for London

Policy 3.1 - Ensuring Equal Life Chances for All

Policy 3.18 - Education Facilities

Policy 5.1 - Climate Change Mitigation

Policy 5.2 - Minimising Carbon Dioxide Emissions

Policy 5.3 - Sustainable Design and Construction

Policy 5.7 - Renewable Energy

Policy 5.12 - Flood Risk Management

Policy 5.13 - Sustainable Drainage

Policy 5.21- Contaminated Land

Policy 6.3 - Assessing Effect of Development on Transport Capacity

Policy 6.9 - Cycling

Policy 6.13 - Parking
Policy 7.2 - An Inclusive Environment
Policy 7.3 - Designing out Crime
Policy 7.4 - Local Character
Policy 7.5 - Public Realm
Policy 7.6 - Architecture
Policy 7.8 – Heritage assets and archaeology
Policy 7.3 - Designing Out Crime
Policy 7.15 – Reducing and managing noise, improving and enhancing the acoustic environment and promoting appropriate soundscapes
Policy 7.18 - Protecting Open Space and Addressing Deficiency
Policy 7.19 - Biodiversity and Access to Nature
Policy 7.21 – Trees and woodlands
Policy 8.2 - Planning Obligations
Policy 8.3 - Community Infrastructure Levy

5.6 Lambeth Local Plan (2015)

Policy D2 - Presumption in favour of sustainable development
Policy D4 - Planning Obligations
Policy S1 - Safeguarding existing community premises
Policy S3 - Schools
Policy T1 - Sustainable Travel
Policy T2 - Walking
Policy T3 - Cycling
Policy T4 - Public Transport Infrastructure
Policy T6 - Assessing impacts of development on transport capacity and infrastructure
Policy T7 - Parking
Policy T8 - Servicing
Policy EN3 - Decentralised energy
Policy EN4 - Sustainable design and construction
Policy EN5 - Flood Risk
Policy EN6 - Sustainable drainage systems and water management
Policy EN7 - Sustainable waste management
Policy Q1 - Inclusive environments
Policy Q2 - Amenity
Policy Q3 - Community safety
Policy Q5 - Local distinctiveness
Policy Q6 - Urban design: public realm
Policy Q7 - Urban design: new developments
Policy Q8 - Design Quality: construction detailing
Policy Q9 - Landscaping
Policy Q10 - Trees
Policy Q12 - Refuse/recycling storage
Policy Q13 - Cycle storage
Policy Q15 - Boundary treatments
Policy Q17 - Advertisements and signage

Policy Q22 - Conservation Areas

Policy Q23 - Undesignated heritage assets: local heritage list

Policy Q25 - Views

5.7 Supplementary Planning Documents

- Accessible London: Achieving an Inclusive Environment SPG (October 2014);
- Sustainable Design and Construction SPG (April 2014)
- Control of Dust and Emissions during Construction and Demolition SPG (September 2014)

5.8 Additional Guidance

- Lambeth Refuse and Recycling Storage Design Guide (2013)
- Lambeth Parking Survey Guidance Note
- Larkhall Conservation Area Statement (2012)

6 THE ASSESSMENT

Land use matters and principle of the development

- 6.1 At the national level, planning policy and guidance is firmly in favour of educational provision. In 2011 the DCLG published a policy statement on planning for schools development, which is designed to facilitate the delivery and expansion of state funded schools.
- 6.2 Paragraph 72 of the NPPF (2012) reiterates the objectives set out in the DCLG Policy Statement on Planning for Schools Development. It clearly confirms that the Government attaches great importance to ensuring that a sufficient choice of school places is available to meet existing and future demand.
- 6.3 Policy S1 of the Local Plan (2015) relating to 'Safeguarding existing community premises' advises that the council will support and encourage the most effective use of community premises to address different and changing priorities and needs in the borough, in accordance with agreed strategies where relevant.
- 6.4 Part (c) of London Plan Policy 3.18 (Education Facilities) states that:
- 6.5 *"Development proposals which enhance education and skills provision will be supported, including new build, expansion of existing or change of use to educational purposes. Those which address the current and projected shortage of primary school places and the projected shortage of secondary school places will be particularly encouraged. Proposals which result in the net loss of education facilities should be resisted, unless it can be demonstrated that there is no ongoing or future demand."*
- 6.6 Policy S3 (a) of the Local Plan states that proposals for new primary and secondary schools, or for the extension or expansion of existing schools, will be supported where they help to deliver the councils agreed strategy for provision of additional state funded

school places in the borough. Submitted documents reveal that the proposed school will accommodate all of its current 420 pupils.

- 6.7 Part (d) of Policy S3 states that proposals for the shared use of schools for wider community use will be supported provided that the nature and scale of the proposal does not unacceptably harm the amenity of the area. In turn part (e) outlines that proposals should support sustainable travel, including the need for school travel plans and appropriate road danger reduction measures.
- 6.8 The proposed development in this case seeks the demolition of the existing main school building and caretakers lodge and the erection of a new part 2, part 3 storey building towards the north of the site, adjacent to Studley Road. The existing music block and nursery are to be retained as existing. The applicants have stated that the existing main school building is in a poor condition and is no longer fit for purpose. The proposal will allow the school to operate from modern, efficient and for for purpose accommodation.
- 6.9 The proposed part two, part three storey school building would cover a reduced footprint of 1,101sqm resulting in a larger external play space of 8864sqm to the rear of the building; however, significantly it should be noted that it would not result in the loss of any school pupil numbers or staff numbers which would be maintained within the rationalised and improved school layout as a result if the increased building scale. In this respect, the proposed development accords with planning policy at all levels in that it seeks to address the current poor condition of the school and its improvement to allow enhanced education and skills provision.
- 6.10 As such, it is considered that the principle of redevelopment of the site for educational purposes is acceptable in land use terms, in accordance with the provisions of Policy S3 of the Local Plan and Policies 3.16 and 3.18 of the London Plan.
- 6.11 With regard to the proposed community use of school facilities, it is considered that a planning condition could be attached to the planning permission requiring the submission of a Community Use Plan (Condition 30).

Design, Conservation & Heritage matters

- 6.12 Paragraph 56 of the NPPF (2012) promotes high quality and inclusive design, recognising that good design is a key aspect of sustainable development. Paragraph 57 goes on to state that, inter alia, developments should function well and add to the overall quality of the area, establish a strong sense of place and respond to local character and history. Policy 7.6 of the London Plan (MALP) states that buildings should be of the highest architectural quality.
- 6.13 Local Plan Policy Q5 requires development to reflect the local distinctiveness of the area and states that proposals should respond to the positive aspects of the local context in terms of urban block and grain, pattern of space and relationship, townscape/landscape character; built form (bulk, scale, height and massing) including roof-scapes; siting, orientation and layout and relationship with other buildings and

spaces; materials; and quality of architectural detailing. Where development proposals deviate from locally distinct development patterns, applicants will be required to show in their design statements that the proposal clearly delivers design excellence; and will make a positive contribution to its local and historic context.

- 6.14 Policy Q7 of the Lambeth Local Plan seeks new development of a high quality design, with a bulk, scale/mass, siting, building line and orientation which preserves or enhances the prevailing local character. The policy also requires that the development is built of durable, robust, low-maintenance materials; includes well considered windows and doors, and any vehicular access, parking or servicing is designed so as to be well related to the adjacent area and minimise impact on amenity and be visually attractive.
- 6.15 Part (a) of Policy Q8 relating to Design quality: construction detailing states that:
- (a) When negotiating schemes the council will seek to ensure that proposed building designs and submitted details are buildable and visually attractive. Poorly-detailed and undeliverable built forms will be resisted.
- 6.16 The proposed development is sited opposite Lark hall Conservation Area and adjacent to Grade II and locally Listed Buildings. As such, Policy Q22 (a) of the Local Plan (2015) considers that development proposals affecting conservation areas will be permitted where they preserve or enhance the character or appearance of conservation areas by i) respecting and reinforcing the established, positive characteristics of the area in terms of the building line, siting, design, height, forms, materials joinery, window detailing etc; (ii) protecting the setting (including views in and out of the area). In addition, Policy Q22 (b) (ii) supports development if a suitable replacement has been granted planning permission.
- 6.17 Policy Q20 of the Local Plan (2015) considers that development affecting listed buildings will be supported where it would: i) conserve and harm the significance/special interest; (ii) not harm the significance/setting (including views to and from; and (iii) not diminish its ability to remain viable in use in the long term.
- 6.18 Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that when considering whether to grant planning permission for development which affects a listed building or its setting, the LPA shall have special regard to the desirability of preserving the building or its setting, or any features of special architectural or historic interest which it possesses.
- 6.19 Section 72 of the Act requires that special attention is paid to the desirability of preserving or enhancing the character or appearance of conservation areas. In this context, "preserving", means doing no harm. Paragraph 132 of the NPPF states 'when considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation. The more important the asset, the greater the weight should be. Significance can be harmed or lost through alteration or destruction of the heritage asset or development within its setting. As heritage assets are irreplaceable, any harm or loss should require clear and convincing justification...'.
- 6.20 Paragraph 134 goes on to state 'where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should

be weighed against the public benefits of the proposal, including securing its optimum viable use.

The existing site

- 6.21 The school site is made up of a number of low rise structures. The school building itself sits centrally on the site and consists of a sprawling, predominantly 2 storey, red brick building. To the south of the site the school has six lower school classrooms with direct access to the play area. The play centre room, music hut and separate nursery class are also situated at this end of the school. At the opposite end there are eight junior classrooms, an art and photocopying room, computer room, library and resources area. There are several playground areas, as divided by the layout of the school building. The caretaker's house, music building and nursery and separate 1 storey constructions, also red brick.
- 6.22 The site is bounded on all sides by a blue painted metal fence atop a low brick wall base. At present there are six pedestrian entry points, however only two are regularly used, these are the entrances at Paradise Road and Larkhall Lane. The fencing is also used to separate areas within the school, with use of manual and electronic gates. The main visible open street frontage of the school is towards Studley Road, to the north of the site, whilst the principle school entrance is located off Paradise Road to the east of the site.

The proposed development

Siting & layout

- 6.23 The applicants have confirmed that the buildings to be demolished include the part one, part two storey existing main school building and the caretakers lodge (a total of 2797.8sqm GIFA). The new part two, part three storey school building would cover a reduced footprint of 1,101sqm resulting in a larger external play space of 8864sqm to the rear of the building. The total GIFA of the new building is 2350.9sqm.
- 6.24 The new school building has been designed in a simple "L" shape profile, with a double height two storey main hall located towards the west of the site. The three storey building, which would cover the bulk of the width of the Studley Road frontage, would accommodate the majority of the teaching accommodation. The Studley Road frontage would provide the main face of the proposed school building, whilst the front building line would reflect that of the nearby flank and front building lines further east along Studley Road and would also retain a 6.0m separation gap to the front site boundary treatment to soften up the scale of the building and allow for landscaping to the front in order to facilitating a welcoming environment along the Studley Road frontage.
- 6.25 The reconfiguration of the school building would allow for the provision of a larger playground towards the south of the site, allowing more open space for school children. Two new entrance points, one for visitors and one for reception pupils, will be created on Studley Road. The main pedestrian pupil access would continue to be

taken from Paradise Road, with access to the nursery gained via a separate gate on Paradise Road. Access to the car park would continue to be from Larkhall Road. Staff car and cycle parking is maintained to the west of the site, via Larkhall Lane, as existing. Deliveries would also come via this entrance. A footpath would lead from the car park to the school.

- 6.26 Internally, the building layout has been designed with a simple and legible layout, with all rooms accessed via a single central corridor, which is flanked on both ends by the stair cores. The school and community benefit from having a large hall, small hall and studio that can be used for external groups at weekend and evenings through a locking system that restricts access to the rest of the school.
- 6.27 It is considered that the layout of the building and external spaces, would allow for optimal use of the site and afford a good environment for study and play, whilst the simple central core layout is legible and easy to access for the wide range of users.

Bulk, scale and height

- 6.28 With regard to site context, the surrounding built environment varies greatly in terms of the building scales, typology and design. For example, Mobey Court which is located on the opposing side of Studley Road measures four storey in height. To the east along Paradise Road are residential blocks ranging between four and five storeys in height, whilst Larkhall Lane varies with some four storey flatted developments and the Victorian and Georgian two/three storey terraced houses which are situated to the north-east of the school site.
- 6.29 The proposed increase in building scale from the current part one, part two storey school to the proposed part two, part three storey scale of development is considered to be acceptable given the surrounding context. In this respect, the proposal would be in keeping with the height, mass and scale and respectful of the neighbouring built environment. Overall the design of the new school building is considered to be of an appropriate bulk, massing and scale, which would give the school increased presence in the area given its civic function, without resulting in an overly dominant development.

External appearance

- 6.30 The proposed school building would be visually broken by a series of three recesses within the Studley Road frontage which would help soften the bulk and massing whilst also adding visual interest. A front entrance canopy with the school name discreetly positioned above in individual lettering would be added to the Studley Road frontage. With regard to materials, the use of brick-slips within the lower element and fibre cement panels above are considered to be appropriate materials that would be robust to withstand weathering, whilst also being in keeping with the brickwork found nearby, especially along Larkhall Lane. Depth to the main elevations is achieved by window reveals, and oversailing the fibre cement between ground floor and above to create a subtle shadow gap. Further texture is added through the use of louvres, which allows the colours of the school hall to weave through the entire scheme.

- 6.31 The smaller scale hall element would be clad in a green coloured cladding (which would have a smooth matt finish) along with a rougher finish dark grey fibre cement cladding, including where it wraps around the flank and rear facades. While the exact final colour scheme of the proposed cladding can be secured by way of planning condition, at this stage the applicants have confirmed that the selected colouration scheme relates to the specific green itself of the nearby nature reserve, and that the proposed louvres which would be set within the recessed window reveals would also match the Marley Eternity cladding colours.
- 6.32 The school hall element (which is a double height space internally) would be designed so that the architectural composition, whilst different to the part three storey element would not be ready as a completely different visual element. This would be achieved by terminating the flat roof element of the school hall in line with the lower window cill level of the main building at second floor level, whilst lower level windows would reflect the height of the corresponding windows within the main block. It is considered that where taken together or as separate elements within the architectural composition, the hall and main school block would be of high quality external finish that would add visual interest to the street scene whilst also ensuring that the character and appearance of the local area is preserved and enhanced.
- 6.33 In terms of the rear of the building, a series of both vertical and horizontal windows would face the main playground area, providing adequate natural light and ventilation whilst also ensure that there is a coherent vertical emphasis to the fenestration. The use of flat roof profiles across the two and three storey elements reflects the existing school building and the nearby residential blocks to the north and east of the site, thereby ensuring that the proposed school building respects the local distinctiveness of the setting.



Figure 5 is a CGI image of the proposed school from Studley Road

- 6.34 With regard to the school boundary treatment, a balance has been sought by the proposals in relation to the need to ensure that the school pupils, staff and visitors are

adequately safeguarded whilst also ensuring that the treatment is sensitive to and respectful of the neighbouring local character.

6.35 The majority of the site would be bounded by the existing metal fencing atop a low brick wall. The total height of this existing fence is around 2m high. An area directly in front of the school would be opened up by adding a lowered metal fence atop the brick wall, to a total height of 1.5m. The applicants have confirmed that there would be gates that can be controlled by the school as they see fit. The use of a perimeter fence provides safety, reassurance and security yet does not block the school from view. A supporting letter from the school and EFA outlines the safety concerns and preferences in regards to the boundary treatment and the rationale behind the design.

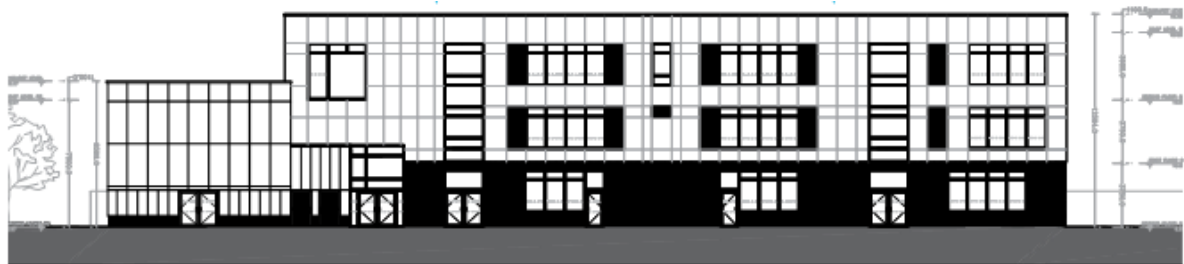


Figure 6 Proposed south elevation

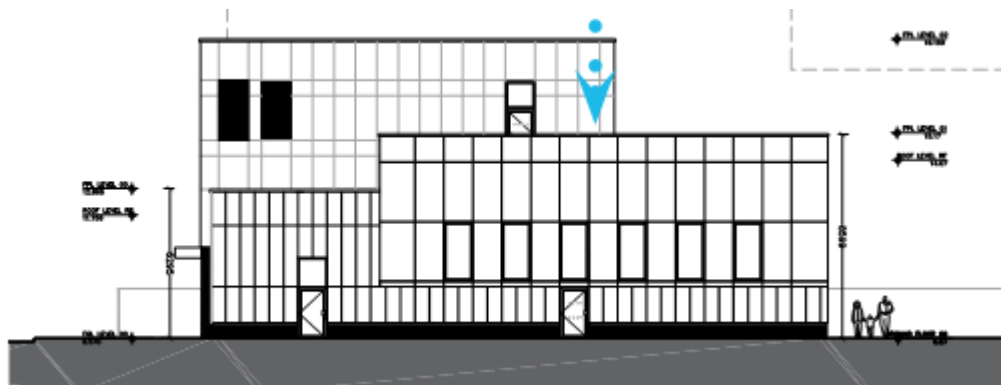


Figure 7 Proposed west elevation



Figure 8 Proposed east elevation

Landscaping

6.36 The redevelopment of Allen Edwards has also presented the school with the opportunity to undertake a comprehensive landscape strategy for the site which can improve on the existing low quality predominantly hard-landscaped play areas, introducing high quality formal and informal play areas for the range of the schools children. In this respect a series of landscape improvements are proposed within the school grounds which include:

- Proposed sand and water play pit;
- Relocation and orientation of benches
- Playground resurfacing with markings;
- Installation of planting beds and amenity grass;
- Improved drainage strategy;
- Wildflower planting to the secret garden;
- Existing shelters to be used as safe and secure buggy storage;
- Cycle and scooter parking improvements;
- Rationalised marked sports pitches within the rear playground;
- Rationalised bin storage.

6.37 The proposals would involve the loss of 8 trees to facilitate the proposed new school building and associated landscape works. However, it is considered that the proposals have maximised opportunities for greening, such as through re-planting of trees and other soft landscaping and have therefore avoided piecemeal treatments and leftover spaces, in accordance with Policy Q9 of the Local Plan (2015). It should also be noted that the Councils' Parks and Open Spaces Officer and the Arboriculture Officer has raised no objections to the details provided. The detailed assessment is also explored further below.

Heritage assessment

6.38 Policy Q22 of the Council's Local Plan (2015) seeks to promote high quality design that makes appropriate reference to the character and appearance of conservation areas and contributes positively to its surroundings.

6.39 The school is not a listed building and it is not located within a conservation area. However, the site sits adjacent to the Larkhall Conservation Area, which runs on the northern side of Larkhall Lane parallel with the school boundary. In addition, listed buildings within the surrounding area include the Grade II listed 76 and 78 Larkhall Lane, and the locally listed 88-94 Larkhall Lane. The site is also located within a Local View Protected Vista for Brixton Historic Towers and Brixton Panoramic, located towards the North West.

Legislative framework

6.40 The following legal commentary is provided:

- 6.41 Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 (“PLBCAA”) provides that in considering whether to grant planning permission for development which affects a listed building or its setting, the local planning authority shall have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses.
- 6.42 Section 72(1) PLBCAA provides that in the exercise, with respect to any buildings or other land in a conservation area, of any functions under or by virtue of (amongst others) the Planning Acts, special attention shall be paid to the desirability of preserving or enhancing the character or appearance of the conservation area.
- 6.43 The South Lakeland District Council v Secretary of State for the Environment case and the Barnwell Manor case (East Northamptonshire DC v SSCLG) establish that “preserving” in both s.66 and s.72 means “doing no harm”.

National Policy

- 6.44 Paragraph 17 of the NPPF (2012) sets out 12 “core planning principles” that should underpin both plan-making and decision-taking. Those principles include the following: “Planning should always seek to secure high quality design” and should “conserve heritage assets in a manner appropriate to their significance so that they can be enjoyed for their contribution to the quality of life of this and future generations”.
- 6.45 The NPPF defines a “heritage asset” as:
- “A building, monument, site, place, area or landscape identified as having a degree of significance meriting consideration in planning decisions, because of its heritage interest”.
- 6.46 The definition includes both designated heritage assets (of which, Listed Buildings and Conservation Areas are relevant here) and assets identified by the local planning authority (including local listing).
- “Significance” is defined within the NPPF as being:
- “The value of a heritage asset to this and future generations because of its heritage interest. That interest may be archaeological, architectural, artistic or historic. Significance derives from a heritage asset’s physical presence, but also from its setting”.
- 6.47 Paragraph 129 of the NPPF requires local planning authorities to identify and assess the particular significance of any heritage asset that may be affected by a proposal (including by development affecting its setting), taking account of the available evidence and any necessary expertise. That assessment should then be taken into account when considering the impact of the proposal on the heritage asset, to avoid or minimise conflict between the heritage asset’s conservation and any aspect of the proposal.
- 6.48 Paragraphs 131 and 132 of the NPPF provide as follows:
- In determining planning applications, local planning authorities should take account of:

- the desirability of sustaining and enhancing the significance of heritage assets and putting them to viable uses consistent with their conservation;
- the positive contribution that conservation of heritage assets can make to sustainable communities including their economic vitality; and
- the desirability of new development making a positive contribution to local character and distinctiveness.

- 6.49 When considering the impact of a proposed development on the significance of a designated heritage asset, great weight should be given to the asset's conservation. The more important the asset, the greater the weight should be. Significance can be harmed or lost through alteration or destruction of the heritage asset or development within its setting. As heritage assets are irreplaceable, any harm or loss should require clear and convincing justification. Substantial harm to or loss of a grade II listed building, park or garden should be exceptional.
- 6.50 Substantial harm to or loss of designated heritage assets of the highest significance, notably scheduled monuments, protected wreck sites, battlefields, grade I and II* listed buildings, grade I and II* registered parks and gardens, and World Heritage Sites, should be wholly exceptional.
- 6.51 Paragraph 133 of the NPPF deals with substantial harm to or total loss of significance of a designated heritage asset.
- 6.52 Paragraph 134 of the NPPF provides that where a development proposal will lead to less than substantial harm to the significance of the designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.
- 6.53 The Larkhall Conservation Area was designated in June 1981 and subsequently extended in July 1983. In 2001 the Angell Arms Public House, Binfield Road was added to the Conservation Area and in 2002 properties fronting Wandsworth Rd were transferred into the newly designated Wandsworth Road Conservation Area (CA59).
- 6.54 The Larkhall Conservation Area statement notes that *'today the conservation area is an L shaped designation following Lansdowne Way and Larkhall Lane. It is characterised by a historic road pattern and 19th Century buildings / urban grain that has survived relatively unscathed in comparison to the wider area which contains much mid-late 20th Century redevelopment. Its slow, evolutionary development has resulted in a pleasant, informal townscape and is worthy of every effort to preserve its special character and appearance.'*
- 6.55 It is considered that the proposals would preserve and enhance the character and appearance of the adjoining Larkhall Conservation Area and would preserve the setting of the Statutory (76 and 78 Larkhall Lane) and locally listed (88-94 Larkhall Lane) buildings nearby within Larkhall Lane on the basis that the proposed school building would be adequately separated from these nearby heritage assets and would result in an improvement from that of the existing school building structures. Furthermore, the proposals would also:
- Result in a clear and consistent building line on the Studley Road elevation;

- Use materials that are respectful of the mixed local character, with the brickwork in particular picking up on that of Larkhall Lane;
- Rationalise and improve the school boundary treatments;
- Improve the surface treatments across the school site and include landscape improvements.

6.56 In addition, the listed buildings within the surrounding area include the Grade II listed 76 and 78 Larkhall Lane, which were listed on 27th August 1976. The listing states that:

Early-mid C19 pair, each two storeys and basement, one window in main block and a slightly lower, set back outer entrance bay. Stock brick with stone-coped parapet recess on axis. Stucco ground floor band. Gauged brick arches to sash windows (some with glazing bars), round headed in round-arched recesses on ground floor. Similar recess to six-panel door with cornice head and patterned radial fanlight in wood classical porch set in angle. The locally listed 88-94 Larkhall Lane are also situated on the western side of Larkhall Lane.

6.57 It is considered that the proposal is acceptable in terms of scale, mass and detailed design and would suitably respond to the adjacent Larkhall Conservation Area and designated and non-designated heritage assets owing to the separation distances between the site and the heritage assets. As such, it is considered that the proposal adheres with the provisions of Policies Q5, Q7 and Q8 of the Local Plan (2015) in this respect.

6.58 It is also considered that the proposal would preserve the character and appearance of the Larkhall Conservation Area. As such, the proposals would adhere with the provisions of Policy Q22 of the Local Plan and Part 12 of the NPPF.

Amenity

6.59 Local Plan Policy Q2 advises that development will be supported if acceptable standards of privacy are provided without a diminution of the design quality; adequate outlooks are provided avoiding wherever possible any undue sense of enclosure or unacceptable levels of overlooking (or perceived overlooking); appropriate levels of daylight and sunlight are provided for existing and proposed occupiers and adequate outdoor amenity space is provided free from excessive noise or disturbance.

Visual Amenity – Sense of Enclosure

6.60 With regard to the site context, it should be noted that the following separation distances would be maintained to the surrounding built environment:

- 28m to the north of the school site with Mobey Court (residential block of flats)
- 16m to the east of the school site with Coade Court (residential block of flats)
- 100m to the south of the school site across the playing areas to the nearest residential blocks
- Between 54m and 63m to the west across Larkhall Lane (this difference accounts for the two storey school hall element with the larger three storey school building set back by 63m from the nearest residential properties along Larkhall Lane).

6.61 Given these large separation distances, and the scale of the proposed building which would be confined to a part two, part three storey scale, it is considered that the neighbouring properties would not suffer any harmful increased sense of enclosure or loss of outlook. With regard to Coade Court to the east, it is considered that given the presence of school, playground that separates the two respective sites (by 16m at the

closest point), it is considered that despite the increased scale, the proposed building massing and bulk in this current case would not appear unduly overbearing.

Daylight and Sunlight

6.62 In support of the application, a daylight and sunlight report has been submitted. This quantitative assessment has been carried out using methodologies set out in the Building Research Establishment (BRE) guide 'Site, Layout and Planning for Daylight and Sunlight (second addition).

6.63 The BRE has developed a series of tests for daylight, which if all are failed, the development would be considered unacceptable in terms of loss of daylight to neighbouring properties. One of the tests used in this case is the Vertical Sky Component (VSC), which measures the amount of available daylight from the sky received at a particular point on a window pane. The BRE guide states that *"if the Vertical Sky Component, with the new development in place, is less than 27%, and less than 0.8 times its former value, then occupants of the existing building will notice a reduction in the amount of skylight"*.

6.64 The BRE has also developed tests for sunlight. In this respect, the BRE Report advises that new development should take care to safeguard access to sunlight for existing buildings and any non-domestic buildings where there is a particular requirement for sunlight. In summary, the report states:

"If a living room of an existing dwelling has a main window facing within 90 degrees of due south, and any part of a new development subtends an angle of more than 25 degrees to the horizontal measured from the centre of the window in a vertical section perpendicular to the window, then the sun lighting of the existing dwelling may be adversely affected. This will be the case if the centre of the window:

- Receives less than 25% of annual probable sunlight hours, or less than 5% of annual probable sunlight hours between 21 September and 21 March and
- Receives less than 0.8 times its former sunlight hours during either period and
- Has a reduction in sunlight over the whole year greater than 4% of annual probable sunlight hours"

6.65 The report submitted with this latest planning application has analysed the neighbouring properties which include:

6.66 **1-15 Parker Court** which is situated to the north of the site. The submitted BRE assessment demonstrates that all windows would meet the BRE guidance.

6.67 **1 – 37 Mobey Court** which is a mid-rise block of residential flats to the north of the application site. The development would pass the '25 degree test' to this block of flats which is positioned 28m away from the front building line of the proposed school building.

6.68 The BRE Daylight analysis using the Vertical Sky Component (VSC) test shows that 94 out of the 99 windows tested (95%) would satisfy the BRE Report guidelines for daylight access. Where transgressions occur (windows W8, W9, W17 at first floor and W6 and W7 at ground floor), they are to windows located beneath balconies. The daylight availability is restricted by this design feature which makes them susceptible to ratio reductions beyond the recommended 0.80, however, the actual quantum of light loss is small with the maximum being 5.66%. The BRE Report states that where an existing property has balconies above windows, a further test can be undertaken

for these windows with the balconies removed. This further test illustrates that all windows would meet the BRE Report guidelines demonstrating that it is the balconies rather than the Development that is restricting the available daylight.

- 6.69 **1-61 Coade Court** which is a four to six storey block of residential flats located to the east of the school site. The submitted BRE assessment demonstrates that all windows would meet the BRE guidance.
- 6.70 **58 Larkhall Lane** is a two storey (plus semi-basement) residential property that is located to the north-west of the school site. This property would adhere with the BRE 25-degree test and no loss of daylight or sunlight has been identified.
- 6.71 In summary Officers are therefore satisfied that the development would comply with the BRE sunlight/daylight criteria. In this respect, Officers have scrutinised the submission and agree with the conclusions of the report. It is considered that this is indicative of an acceptable building massing and bulk for the site, in accordance with the provisions of Local Plan Policy Q2.

Privacy

- 6.72 It is considered that the orientation of the proposed school building and the relative large separation distances to neighbouring residential properties would safeguard against any potential loss of privacy or increased overlooking for neighbouring properties. In the case of Coade Court, it is again considered that the minimum separation distance of 16m would adequately safeguard neighbouring amenity given this separation distance across the school yard and given that the window sizes within that particular flank elevation are limited in order to restrict views from the primary school classrooms. As such, it is considered that the proposal would be in accordance with Policy Q2 of the Local Plan (2015).

Noise and disturbance

- 6.73 The application site is already in use as a school (Class D1 use). As the development is not intensifying the use of the site for educational purposes (with the proposals improving the quality of the school not quantum) it is considered that the additional noise and disturbance would not be unduly harmful to the living conditions of neighbouring surrounding residential properties. The proposed playground would be situated in the central section of the site and in a similar location to that of the existing playground, and whilst the playground size would be increased, the school pupil numbers would not, thereby no additional noise generating sources have been identified. Should permission be granted it is recommended that a condition is imposed preventing amplified sound, amplified speech, or amplified music which would be audible outside the school building.
- 6.74 Noise from plant and equipment at the site is also a consideration. A detailed Environmental Noise Assessment report has been submitted in respect of the primary school. Regulatory Services have been consulted and have raised no objection to the proposal subject to the use of conditions, in accordance with Policy Q2 of the Council's Local Plan (2015).

Transport

- 6.75 The NPPF (2012) seeks to promote sustainable transport and in doing so it seeks to ensure that new development which generates significant movements are located where the need to travel would be minimized and the use of sustainable modes can be

maximized. This is echoed by policies 6.1 and 6.3 of the London Plan (MALP, 2016) which also seeks to ensure that the impacts of development in transport capacity and the transport network are fully assessed.

- 6.76 Policies T3, T6, T7 and T8 of the Local Plan (2015) seek to ensure that proposals for development have a limited impact on the performance and safety of the highway network and that sufficient and appropriate car parking and cycle storage is provided whilst meeting objectives to encourage sustainable transport and to reduce dependence on the private car. If development would have an unacceptable transport impact, it should be refused in the absence of mitigation measures to make the development acceptable.
- 6.77 Policy T1 of the Lambeth Local Plan states that the Council will promote a sustainable pattern of development in the Borough, minimising the need to travel and reducing dependence on the private car.
- 6.78 Policy T6 states that planning applications will be supported where they do not have unacceptable transport impacts including cumulative impacts upon highway safety; traffic flows; congestion of the road network; on-street parking; footway space, desire lines and pedestrian flows; all other transport modes, including public transport and cycling unless contributions or improvements are to be provided that would make the impact acceptable.

Pupil and staff numbers

- 6.79 There is no planned pupil expansion for the school and the school will remain at 420 pupil places. Staff numbers will not increase from the existing 46, resulting from the proposed replacement school building. The application is supported by a comprehensive Transport Assessment (TA), which quantifies the impact of the proposed development, including a breakdown of the specific impacts associated with the primary school.

Accessibility

- 6.80 The site is highly accessible by public transport and has a PTAL rating of 4-5 which equates to 'Good to Very Good' with regard to levels of public transport accessibility, therefore the principle of development is considered to be acceptable and in principle the Council's Transport Officer raises no objections. The school currently has three access points on Paradise Road, Studley Road and Larkhall Lane. The main entrance for children, staff, parents and visitors is on Paradise Road, with a secondary entrance on Studley Road, while the Larkhall Lane entrance is for vehicles only.
- 6.81 The proposed school building will be reoriented so that reception and visitor access is taken from Studley Road, while the main primary pupil access will continue to be taken from Paradise Road. Vehicular access is to be retained from Larkhall Lane to provide access into the existing car park.

Car parking

- 6.82 By way of context, the site is within Controlled Parking Zone (CPZ) Stockwell 'S' which is operational Monday-Friday 08:30-17:30. The school currently has a small staff car park located towards the west of the site, with access via Larkhall Lane. There are a total of 23 parking spaces, and an additional 2 Blue Badge holder parking spaces. Vehicular access is to be retained from Larkhall Lane to provide access into the existing car park, which is also to be retained as part of the developments proposals.

6.83 The applicant has confirmed that the proposed primary school layout will retain the location of the existing dedicated staff car park and will provide 21 standard and 2 disabled parking spaces. This is to be managed by the school to prevent staff parking on-street or obstruction of the main carriageway during peak arrival and departure periods. It is considered that further details of this management could be secured by way of an updated Travel Plan by way of condition and the monitoring by way of S106 agreement. The Council's Transport officers have reviewed the proposal and offer no objections to the scheme.

Cycle provision

6.84 Policy Q13 of the Council's Local Plan (2015) relates to Cycle Storage and states that:

(a) Cycle storage in all development (including conversions) should be:

- (i) Fully integrated into proposals from the initial design stage;
- (ii) Directly and conveniently accessed from outside the building;
- (iii) Inclusive, secure and safe to use;
- (iv) In individual lockers/cupboards, or if necessary, in cycle stores shared by small groups of immediate neighbours; and
- (v) Covered, fully ventilated, robustly constructed and easy to maintain.

6.85 The school currently has 12 cycle parking spaces for both staff and pupils. This level of cycle parking falls well short of the level of cycle parking required by the London Plan (2016), which suggests the school should have a total of 63 cycle parking spaces for both staff and pupils.

6.86 A total of 36 secure pupil cycle parking spaces would be made available on site, 12 of which will be covered. They would be located in between the school building and the boundary fence to Studley Road with a further 4 spaces for visitors in that same location. 12 covered staff secure cycle parking spaces would be made available within the staff car parking area, whilst 24 pupil spaces would be made available in the south-east corner of the site (along with 18 scooter spaces which are also proposed).

6.87 Officers, along with Lambeth Transport have reviewed the quantum and quality of the provision that has been proposed in this case and note that whilst this level does not meet the London Plan standard, it is a significant improvement on the existing level of cycle parking, and it's understood that a safeguarded area for the additional 15 spaces is being provided, furthermore, the Travel Plan will also include monitoring of the use of cycle parking. As such, this approach is considered acceptable, subject to further details being secured via condition. The proposals are therefore considered to adhere with the provisions of Policy Q13 of the Local Plan (2015).

Traffic impacts

6.88 The existing 2FE school has 240 pupils on the roll, including 40 nursery pupils. It's understood that the school will remain at 420 pupil places. Staff numbers are also not expected to increase from the existing 46. The catchment of the school is not expected to change as a result of the proposals. Therefore, the traffic impacts are expected to be negligible.

Servicing and deliveries

- 6.89 Policy T8 of the Lambeth Local Plan (2015) states that new development, and in particular on residential and mixed-use development, will be permitted only where adequate provision is made for servicing appropriate to the scale, form and location of the proposed development.
- 6.90 It is proposed that a kitchen will be provided in the new school building accessed from Studley Road. A 55sqm bin store is being provided adjacent to the kitchen. The Framework Delivery and Servicing Plan suggests that refuse collection vehicles and delivery vehicles will stop in residents' bays on Studley Road to service the site and notes that delivery vehicles can also enter the site from Larkhall Lane.
- 6.91 The applicant has also confirmed that Allen Edwards Primary School management have indicated that the following delivery vehicles currently and will continue to visit the site at the following frequencies:
- Fruit and Milk delivery van* - Once per day;
 - Refuse Collection Vehicle - Twice per week
 - Recycling Collection Vehicle - Twice per week
 - Kitchen delivery van - Twice per day
 - After-school club food delivery van* - Twice per week
- 6.92 It is also understood that the school delivers food to Sudbourne School and St Marks Church of England School and that this will continue following the redevelopment. The school will thus be receiving deliveries of food to meet the demand of Allen Edwards, Sudbourne and St Marks Church of England Schools. There will also be daily collections of food supplied to Sudbourne and St Marks Church of England Schools. At present, these servicing vehicles service the site from the on-site car park accessible via the Larkhall Lane entrance.
- 6.93 The applicant has confirmed that following the redevelopment, it is anticipated that servicing vehicles will normally service the site from the same location, or from the play area beyond via the subsequent gate when this area is not in use. The ability of standard delivery vehicles to access this area is demonstrated by the swept path analysis provided in Appendix D of the submitted Transport Statement, which demonstrates that a fire tender which is both larger and less manoeuvrable than standard delivery vehicles (3.5 / 7.5 tonne) can enter, turn and exit this area in a forward gear.
- 6.94 It is proposed that refuse vehicles service the site from kerbside on Studley Street. This will allow refuse collectors to reach within 25m of the bin store provided in the north-eastern corner of the site. Refuse from the bin store will be removed from the site using a new double gate adjacent to the existing guard-railing. A dropped kerb is also suitably located to permit sufficient access by refuse collectors. In summary, whilst no objections are raised, further information is required on the servicing strategy, in particular how the servicing strategy is proposed to change from the existing situation, this will be secured by way of condition.
- 6.95 Policy T8 (d) of the Local Plan requires planning applications to be accompanied by a construction and logistic plan, demonstrating arrangements for construction traffic and how environmental, traffic and amenity impacts would be minimised. A Construction Management Plan would be secured as a condition of consent, detailing how the construction of the development would be managed including measures for mitigating

construction vehicle movements and for ensuring minimal nuisance or disturbance is cause the amenity of nearby residential occupiers and the area generally.

Travel plan statement

- 6.96 The school has an existing Travel Plan, however, following review Transport Officers have concluded that this is quite out of date (latest travel survey conducted in 2010), and it is understood that the school does not currently engage in the STARS programme. The Framework Travel Plan included in the submitted Transport Assessment (TA) suggests that a revised full Travel Plan will be submitted after occupation of the new building. This will not be acceptable and a full Travel Plan will be secured via condition, prior to occupation. In addition, the Travel Plan could include more focus on encouraging cycling, from the current low base, and also to take advantage of the Cycle Quiet way. As such, in accordance with Polies D4 and T1, the s106 requires a fee for the monitoring of the Travel Plan, which itself would be secured by way of condition.

Construction impacts

- 6.97 Policy T8 of the Lambeth Local Plan (2015) states that planning applications for major developments should include a Construction Management and Logistics Plan that is appropriate to the scale of the development, demonstrating arrangements for construction traffic and how environmental, traffic and amenity impacts will be minimised.
- 6.98 The application site adjoins residential properties and has a highly visible and wide frontage to Studley Road and activities have potential to impact on the area including the amenity of nearby residential properties, on the operation of the local highways network and on the general amenity of the area. To address these impacts and to ensure the external appearance of the application site is satisfactory during the construction period, officers recommend a condition to secure a detailed Method of Construction Statement which provides full details of the proposed construction methodology. The statement should advise how neighbours and adjoining businesses will be notified of any works, all reasonable measures to protect residential amenity.

Waste and recycling

- 6.99 Local Plan Policy Q12 (Refuse/recycling storage) notes that adequate refuse and recycling storage should be provided for all development, in order to protect visual and residential amenity and public health. The policy goes on to note that refuse and recycling storage should be large enough to accommodate the easy manoeuvring of refuse/recycling containers and have sufficient space to accommodate any increased storage requirements. A 55sqm bin store is being provided adjacent to the kitchen. The Framework DSP suggests that refuse collection vehicles and delivery vehicles will stop in resident's bays on Studley Road to service the site and delivery vehicles can also enter the site from Larkhall Lane. In terms of design and location, the proposals for refuse/recycling storage meet the Council's waste and recycling storage and collection requirements guidance document (October 2013) and would be compliant with Policy Q13 of the Local Plan relating to refuse and recycling storage. However, it is not known how the school is serviced at present and further detail is required on this point.
- 6.100 The applicants have also confirmed that servicing will be carried out as per the current arrangement, being undertaken from the dual use parking/servicing bays on Lower Marsh. The servicing requirement would be very similar to that of the existing users. A

condition is proposed securing a Waste Management Plan prior to the occupation of the proposed development to ensure that the details of all the servicing / waste provision requirements are detailed, in accordance with Local Plan Policies T6, T8 and Q12.

Planning obligations and CIL

- 6.101 Policy D4 of the Local Plan refers to circumstances in which the Council can seek S106 Planning Obligations to mitigate the impact of development on the local infrastructure or secure additional facilities that are required as a result of the development coming forward.
- 6.102 Officers consider that the following financial contributions should be secured by way of s106 legal agreement to mitigate against the impact of the development in accordance with the requirements of Policy D4.

Travel Plan monitoring fee

- 6.103 The Lambeth and Mayoral CIL contribution would not be payable because of the proposed use of the site as an educational facility.

Sustainability and energy

- 6.104 The NPPF (2012) seeks that development promotes the use of renewable energy where technology is viable, Economic and where the social impacts can be addressed satisfactorily. Policy 5.3 of the London Plan states that development proposals should demonstrate that sustainable design standards are integral to the proposal and should meet the minimum standards set out in the Mayor's 'Sustainable Design and Construction' SPG (2006).
- 6.105 Policy 5.2 of the London Plan (2015, as amended) states that development proposals make the fullest contribution to Minimising carbon dioxide emissions in accordance with the Mayors energy hierarchy, namely; using less energy, supplying energy efficiently and using renewable Energy. The London Plan requires that all major developments meet specific targets for carbon dioxide emissions reduction in buildings. These targets are expressed as minimum improvements over the Target Emission Rate (TER) outlined in the national Building Regulations (2006).
- 6.106 Policy EN4 (Sustainable design and construction) of the Local Plan (2015) requires all development to meet high standards of sustainable design and construction feasible, having regard to the scale, nature and form of the development proposal. Proposals should demonstrate in a supporting statement that these standards are integral to the design, construction and operation of the development. Non-residential developments are required to show how the development would meet the British Research Establishment Environmental Assessment Methodology (BREEAM) target credit rating of at least "Excellent" through the implementation of the London Plan Energy Hierarchy of energy efficiency, decentralized energy and renewable energy technologies (London Plan Policy 5.2 refers).
- 6.107 The applicants have presented a range of active design measures such as the implementation of efficient heating systems, heat recovery, natural cooling systems, efficient ventilation systems, low energy mechanical services and energy efficient lighting which would also contribute to the energy efficiency of the proposed new school building. The Council's Sustainability Consultant was consulted on the proposals and confirmed that the scheme is acceptable in sustainability terms given

that the submitted pre-assessment demonstrates that a compliant score is targeted. However, this must be conditioned, namely that a Design Stage BREEAM assessment is undertaken and submitted to the Planning Authority for their approval prior to the commencement of above ground works showing how BREEAM Very Good and a minimum score of 63% will be achieved in practice, as per Lambeth Local Plan Policy EN4.

Community Safety

- 6.108 Local Plan Policy Q3 states that in order to create a safe borough for all users, the council will expect development to utilise good design to design out opportunistic crime, anti-social behaviour and fear of crime in a site-specific manner, based on an understanding of the locality and the likely crime and safety issues it presents. It also states that applicants should meet the standards and objectives of the Secure by Design initiative.
- 6.109 The MET Police Designing out Crime Officer was consulted on the proposals and has raised no objections stating that the proposal should be built to achieve security specifications required with the guidance of Secured by Design (SBD) (New Schools 2014), and the adoption of these standards will help to reduce the opportunity for crime, creating a safer, more secure and sustainable environment. In this respect, a condition to secure SBD measures is included in the recommendation.

Flood Risk

- 6.110 When determining applications, the NPPF requires local planning authorities to ensure that development does not increase flood risk elsewhere or is properly considered against the relevant tests when located in an area at risk of flooding. Policy EN5 of the Local Plan (part (a)) states that the council will seek to minimise the risk of flooding in the borough through, amongst other things, permitting appropriate development in Flood Zones 1,2,3a and 3b subject to meeting the criteria set out in Annex 5 of the Local Plan. Part (b) of the policy states that all development in Flood Zones 2, 3a and 3b defined in the Strategic Flood Risk Assessment, or identified as at risk of flooding from other sources, should contribute positively to actively reducing flood risk through avoidance, reduction, management and mitigation.
- 6.111 Policy EN5: Flood Risk(c) of the Local Plan requires all major development proposals to be accompanied by a flood risk assessment (FRA), which should be proportionate with the degree of flood risk posed to and by the development. Policy EN5 (d) requires an FRA to consider the risk of flooding to and from all sources including surface runoff, groundwater, ordinary watercourses, sewer and reservoirs.
- 6.112 The site is located within Flood Zone 1 and since it is over 1ha in size, a Flood Risk Assessment is required. The NPPF (2012) defines Flood Zone 1 as land assessed as having less than a 1 in 1,000 annual probability of river or sea flooding (<0.1%). Therefore fluvial & tidal risk at this site can be considered to be low. Flood risk from other sources including reservoirs, canals, groundwater and sewers have also been assessed to represent a low residual risk to the site.
- 6.113 The submitted Flood Risk Assessment recommends that finished floor levels are raised generally above surrounding ground levels to help mitigate any risk posed from pluvial/sewer flooding. Minor re-profiling of the site is to be undertaken to manage any residual risk and channel any overland flood flows towards the positive drainage system.

- 6.114 The Environment Agency (EA) have been consulted on the application and have reviewed the associated Flood Risk Assessment. The EA have raised no in principle objections. They have commented that they will not be providing extensive comments on flood risk grounds, as the site is situated within Flood Zone 1, which is defined as having a 'low probability' of river or sea flooding. The EA noted that there may also be other sources of flooding which affect this site – such as surface water and groundwater flooding – which are not within our direct remit, but could be important considerations for managing flood risk for the proposed development.
- 6.115 The Council's Flood Risk Officer has also reviewed the application and has noted that generally there is no concern regarding flood risk. The site is not at risk from tidal, fluvial, groundwater or surface water. The impacts of the development will not significantly change this either subject to the use of a condition to secure a final drainage strategy.
- 6.116 As such, and together with the imposition of a condition to secure a final drainage strategy and given the low flood risk identified and the measures proposed within the submitted FRA, Officers raise no objections to the proposal on flood risk grounds, in accordance with Policy EN5 of the Local Plan (2015).

Impact on trees, biodiversity and wildlife habitats

- 6.117 Local Plan policy Q10 states that proposals for new development will be required to take account of existing trees on the site and on adjoining land and that development will not be permitted where it would result in the loss of trees of significant amenity, historic or ecological / habitat conservation value.
- 6.118 Local Plan policy Q9 requires that development retains and enhances existing planting and landscaping features and protects them during construction. It also requires that development protects existing designated habitats and create new habitats / areas of conservation interest and biodiversity value.
- 6.119 The applicants have submitted a detailed Arboriculture Implication Assessment (AIA) document which sets out amongst other matters, a review of categorisation of the trees on the school site and which are to be retained, removed or pruned. Of the 41 individual trees and tree groups surveyed, eight would be lost as a result of the proposed development. Of those that would be lost, 5 are low 'C' Category trees and three are moderate value 'B' Category.
- 6.120 An extensive new planting plan has been compiled, which includes for the planting of multiple new trees and shrubs, including, but not limited to, Silver Birch (*Betula pubescens*) and Wild Cherry (*Prunus avium*). It is considered that the provision of multiple new good quality specimens would improve the long-term viability of the tree stock and mitigate for the loss of existing trees. Furthermore, the applicants have also indicated that mitigation will be required to protect eight of the retained trees/ groups.
- 6.121 The Council's Arboriculture Officer has reviewed the scheme and submitted Arboriculture Implication Assessment and raises no objections subject to the use of conditions to secure a robust Tree Protection Plan. In this respect, it is considered that the proposal adheres with the provisions of policies Q9 and Q10.
- 6.122 With regard to biodiversity, part (B) of Policy EN1 relating to 'Open space and biodiversity' is relevant which states that the council will meet requirements for open space by preventing development which would result in loss, reduction in area or significant harm to the nature conservation or biodiversity value of an open space

including any Designated or proposed Local Nature Reserves (LNR) or Sites of Importance for Nature Conservation (SINC) unless adequate mitigation or compensatory measures are included appropriate to the nature conservation value of the assets involved.

- 6.123 The Council's Parks and Open Spaces Officer has reviewed the application and raises no objections stating that the direct risk to the adjacent Borough Site of Importance for Nature Conservation (Borough SINC), namely the Oasis nature Garden (SINC08, old code LaBII06), is relatively minimal and provided there is no intention to encroach onto or intrude into the SINC, which would result in new build or loss of natural space, then Parks and Open Spaces would be supportive of this application. A condition should be added to ensure that applicants implement all of the key recommendations in the Preliminary Ecological Appraisal. In this light, Officers consider that there would be no impact upon the SINC or on local wildlife and habitats, in accordance with Policy EN1 of the Local Plan.

7 CONCLUSION

- 7.1 The application submission has demonstrated that the proposed development would be acceptable in principle and provide an acceptable quality of educational accommodation. The development is acceptable with regard to its scale, massing, bulk and detailed design, sustainability considerations, ecology and flood risk, secure by design standards, residential amenity considerations, impact on heritage assets and transport and highway implications for the area.
- 7.2 Officers consider that the proposed development would be in compliance with the Local Plan and there are no material considerations of sufficient weight that would dictate that the application should otherwise be refused. Officers are therefore recommending approval of the scheme, subject to conditions and the completion of a Section 106 Legal Agreement.

8 RECOMMENDATION

- 8.1 Resolve to grant conditional planning permission subject to completion of an agreement under Section 106 of the Town and Country Planning Act 1990 for the obligations listed in this report.
- 8.2 Agree to delegate authority to the Assistant Director of Planning, Transport and Development to finalise the recommended conditions and section 106 legal agreement as set out in this report.
- 8.3 In the event that the committee resolves to refuse planning permission and there is a subsequent appeal, delegated authority is given to officers, having regard to the heads of terms set out in the report, to negotiate and complete a document containing obligations pursuant to Section 106 of the Town and Country Planning Act 1990 in order to meet the requirement of the Planning Inspector.

Conditions(s) and Reasons(s)

- 1) The development to which this permission relates must be begun no later than three years from the date of this decision notice.

Reason: To comply with the provisions of Section 91(1) (a) of the Town and Country

Planning Act 1990 (as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.)

- 2) The development hereby permitted shall be carried out in complete accordance with the approved plans listed in this decision notice, other than where those details are altered pursuant to the requirements of the conditions of this planning permission.

Reason: For the avoidance of doubt and in the interests of proper planning.

- 3) Prior to demolition of any existing buildings on-site full details of the proposed demolition methodology, in the form of a 'Method of Demolition Statement' shall be submitted to and approved in writing by the local planning authority. Subsequently, prior to implementation of the development (other than demolition) full details of the proposed construction methodology in the form of a 'Method of Construction Plan' shall be submitted to and approved in writing by the local planning authority. The Method of Construction Statement shall include details regarding:

- a) The scope and nature of notifying neighbours with regard to specific works;
- b) Advance notification of road closures;
- c) Details regarding parking, deliveries, and storage;
- d) Details regarding dust mitigation;
- e) Details of measures to prevent the deposit of mud and debris on the public highway; and
- f) Any other measures to mitigate the impact of construction upon the amenity of the area and the function and safety of the highway network.

'No demolition or development shall commence until provision has been made to accommodate all site operatives', visitors' and construction vehicles loading, off-loading, parking and turning within the site or otherwise during the relevant phase of the construction period in accordance with the approved details. The demolition and development shall thereafter be carried out in accordance with the details and measures approved in the Method of Construction Statement'.

Reason: Development must not commence before this condition is discharged to avoid hazard and obstruction being caused to users of the public highway, to safeguard residential amenity from the start of the construction process, to ensure the neighbouring heritage asset is sufficiently protected. (Policies T8, Q21, EN1 and Q2 of the Lambeth Local Plan and Policy 7.19 of the London Plan (2011).

- 4) No development above ground level shall take place until detailed drawings of all external construction (detailing to be provided at a minimum scale of 1:10) including copings, cills, reveals, rain water goods, junctions of materials, fixings, soffits and canopies together with samples of all external materials and a colour scheme have been submitted to and approved in writing by the Local Planning Authority.

This condition shall apply notwithstanding any indications to these matters which have been given on approved plans and in the application. The development shall thereafter be carried out solely in accordance with the approved details.

Reason: To ensure an adequate quality of detailed design so as to safeguard and enhance the visual amenities of the locality (Policies Q7, Q8, Q11, Q12, and Q22 of the Lambeth Local Plan).

- 5) No development above ground level shall take place until details (at a minimum scale of 1:50) of the siting and design of all walls, gate and/or fencing and any existing boundary treatment to be retained at the site, have been submitted to and approved in writing by the Local Planning Authority. Such walls or fencing as may be approved shall be erected prior to the first occupation of the new building unless the prior written approval of the Local Planning Authority to any variation has been obtained.

Reason: To ensure an adequate quality of detailed design so as to safeguard and enhance the visual amenities of the locality (Policies Q7, Q8, Q11, Q12, and Q22 of the Lambeth Local Plan).

- 6) Prior the commencement of any above ground works details of the location, design and external appearance of any external mechanical and electrical plant to be installed on the site, including any kitchen extract and PV installations shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in strict accordance with the approved details and any mitigation measures required shall be installed prior to the first occupation of the development.

Reason: To ensure an adequate quality of detailed design so as to safeguard and enhance the visual amenities of the locality (Policies Q7, Q8, Q11, Q12, and Q22 of the Lambeth Local Plan).

- 7) Notwithstanding the approved drawings, and prior to the commencement of the development above ground level, samples (including the invitation to view sample panels on site) and a schedule of all materials to be used in the external elevations of the development hereby approved, including:

All external construction details at 1:10; details of joinery, cladding (including colour schemes), windows, doors, cills, reveals, parapets, copings, air-vents, brickwork, gutters, flues, rainwater goods, extract systems, canopies, details of refuse storage including ventilation and details of mechanical plant, shall be submitted to and approved in writing by the local planning authority. The development shall thereafter be carried out in accordance with the approved details.

Reason: To ensure an adequate quality of detailed design so as to safeguard and enhance the visual amenities of the locality (Policies Q7, Q8, Q11, Q12, and Q22 of the Lambeth Local Plan).

- 8) No plumbing or pipes, other than rainwater pipes, shall be fixed to the external faces of buildings.

Reason: To ensure an appropriate standard of design (Policies Q5, Q7, Q8 and Q22 of the Lambeth Local Plan).

- 9) Notwithstanding any indications to these matters which have been given on approved plans and in the application, details including the layout, siting and elevations of the refuse and recycling stores for the commercial development hereby approved (at a scale of not less than 1:20) shall be submitted to and approved by in writing by the Local Planning Authority prior to the first occupation of the development. The provision for waste storage and recycling shall be made in accordance with the requirements of the London Borough of Lambeth's 'Waste and Recycling Storage and Collection Requirements: Guidance for Architects and Developers' (October 2013). The development shall thereafter be completed in accordance with the approved details and permanently retained as such unless the prior written approval of the Local Planning Authority has been obtained for any variation.

Reason: To ensure the adequate provision is made for the storage of refuse on the site, in the interests of the amenities of the area and to ensure a satisfactory appearance of the completed development (Policies Q2, Q5, Q7, Q12, Q20, Q22, Q23 and EN7 of the Lambeth Local Plan (2015) refers).

- 10) Prior to the occupation of the development hereby permitted, a Waste Management Strategy shall be submitted to and approved in writing by the local planning authority. The development hereby permitted shall be built in accordance with the approved details and shall thereafter be retained solely for its designated use. The use hereby permitted shall thereafter be operated in accordance with the approved Waste Management Strategy. The Waste Management Strategy will align with the guide for architects and developers on waste and recycling storage and collection requirements submitted details and will include the following:

- a) details of refuse store capacity
- b) details of the management of glass disposal including the hours within which it will be transferred to the outside refuse storage.
- c) hours of refuse collection
- d) Details of fat, oil and grease disposal methods.

Reason: To ensure that adequate provision is made for the storage of refuse, the disposal of waste and the provision of recycling facilities on the site and in the interests of the amenities of the area. (Policy Q12 of the Lambeth Local Plan)

- 11) Prior to the commencement of development approved by this planning permission (or such other date or stage in development as may be agreed in writing with the Local Planning Authority), the following components of a scheme to deal with the risks associated with contamination of the site including the presence of asbestos containing materials shall each be submitted to, and approved in writing by, the Local Planning Authority:

1) a preliminary risk assessment which has identified:

- all previous uses;
- potential contaminants associated with those uses;
- a conceptual model of the site indicating sources, pathways and receptors;
- potentially unacceptable risks arising from contamination at the site.

2) a site investigation scheme, based on (1) to provide information for a detailed assessment of the risk to all receptors which may be affected, including those off site;

3) the results of the site investigation and detailed risk assessment referred to in (2) and, based on these, an options appraisal and remediation strategy giving full details of the remediation measures required and how they are to be undertaken;

4) a verification plan providing details of the data collected in order to demonstrate that the works set out in the remediation strategy in (3) are complete and identifying any requirements for longer-term monitoring of pollutant linkages, maintenance and arrangements for contingency action.

Any changes to these components require the express consent of the Local Planning Authority. The scheme shall be implemented as approved.

Reason: To ensure that the development does not impact unacceptably upon existing ground conditions, geology, hydrology and hydrogeology of the site and surrounding area (Policies 5.14 and 5.21 of the London Plan).

- 12) If, during development, contamination not previously identified is found to be present at the site then no further development (unless otherwise agreed in writing with the Local Planning Authority) shall be carried out until the developer has submitted, and obtained written approval from the Local Planning Authority for, a remediation strategy detailing how this unsuspected contamination shall be dealt with. The remediation strategy shall be implemented as approved, verified and reported to the reasonable satisfaction of the Local Planning Authority.

Reason: To ensure that the development does not impact unacceptably upon existing ground conditions, geology, hydrology and hydrogeology of the site and surrounding area (Policies 5.14 and 5.21 of the London Plan).

- 13) The development hereby approved shall not commence until a Construction and Environmental Management Plan (CEMP) has been submitted to and approved in writing by the local planning authority. The CEMP shall observe the requirements of the statutory and other relevant guidance and shall include details of the following measures:

- i. An introduction consisting of construction phase environmental management plan, definitions and abbreviations and project description and location;
- ii. A description of management responsibilities;
- iii. A description of the construction programme which identifies activities likely to cause high levels of noise or dust;

- iv. Site working hours and a named person for residents to contact;
- v. Detailed Site logistics arrangements;
- vi. Details regarding parking, deliveries, and storage;
- vii. Details regarding dust and noise mitigation measures to be deployed including identification of sensitive receptors and ongoing monitoring;
- viii. Details of any out of hours works and other measures to mitigate the impact of construction on the amenity of the area and safety of the highway network; and
- ix. Communication procedures with the LBL and local community regarding key construction issues – newsletters, fliers etc.

The construction shall thereafter be carried out in accordance with the details and measures approved in the CEMP for the related phase, unless the written consent of the Local Planning Authority is received for any variation.

Reason: This is required prior to construction to avoid hazard and obstruction being caused to users of the public highway and to safeguard residential amenity during the whole of the construction period. (Policies T6 and Q2 of the Lambeth Local Plan (2015)).

- 14) Prior to the commencement of the use hereby permitted, a scheme of sound insulation and vibration isolation to protect the educational use shall be submitted to and approved in writing by the Local Planning Authority. As a minimum, the scheme shall meet the noise targets recommended in the Environmental Noise Assessment report prepared by Mach Acoustics dated 17th March 2017. The scheme shall include details of post construction validation. The development shall be carried out in accordance with the approved details. Within 3 months of the date of commencement of the use a noise and vibration assessment shall be carried out to confirm the noise and vibration targets have been met for daytime operation. Any additional steps required to mitigate noise shall be detailed and implemented, as necessary. The post installation assessment shall be submitted to and approved in writing by the local planning authority. The details as approved shall thereafter be permanently retained.

Reason: To ensure the development is suitable for the intended use and to protect the amenities of future residential occupiers and the surrounding area (Policy Q2 of the Lambeth Local Plan (2015)).

- 15) Prior to the commencement of building works above ground of the relevant part of the development, full details of internal and external plant equipment and trunking, including building services plant, ventilation and filtration equipment and commercial kitchen exhaust ducting / ventilation, shall be submitted to and approved in writing by the Local Planning Authority. All flues, ducting and other equipment shall be installed in accordance with the approved details prior to the use commencing on site and shall thereafter be maintained in accordance with the manufacturer's instructions.

Reason: To protect the amenities of future residential occupiers and the surrounding area (Policy Q2 of the Lambeth Local Plan (2015)).

- 16) Noise from any mechanical equipment or building services plant shall not exceed 5 dB below background noise level when measured outside the window of the nearest noise sensitive or residential premises, when measured as a L90 dB(A) 1 hour. Within 3 months of the date of commissioning all equipment and plant a noise assessment shall be carried out to confirm the noise target has been met for both day and night time operation. Any additional steps required to mitigate noise shall be detailed and implemented, as necessary. The post installation noise assessment shall be submitted to and approved in writing by the local planning authority. The details as approved shall thereafter be permanently retained.

Reason: To protect the amenities of future residential occupiers and the surrounding area (Policy Q2 of the Lambeth Local Plan (2015)).

- 17) Prior to any use of the development for purposes other than educational use a Management Plan must be submitted to and agreed by the Local Planning Authority. The plan should include but not be limited to, management responsibilities during all operating hours, measures to control noise and disturbance, playing of music or other amplified sound and minimising the effects of patrons coming and going from the site. Where any management practices give rise to reported concerns of impact to local amenity received by the operator or London Borough of Lambeth, this must be brought to the attention of the Local Planning Authority and any necessary changes to the management plan implemented to the satisfaction of the Local Planning Authority.

Reason: To protect the amenities of future residential occupiers within the surrounding area (Policies Q2 and T6 of the Lambeth Local Plan (2015)).

- 18) There shall be no amplified sound, speech or music used in connection with the commercial premises hereby approved which is audible above background noise levels when measured outside the nearest residential property.

Reason: To protect the amenities of future residential occupiers and the surrounding area (Policy Q2 of the Lambeth Local Plan (2015)).

- 19) The development shall be undertaken in accordance with the Air Quality Assessment prepared by BWB dated 24th March 2017. No variation or amendment of the development shall be permitted until a report is provided that demonstrates the conclusions of the Air Quality Assessment will remain valid. The report shall be submitted to and approved in writing by the Local Planning Authority. Where Air Quality Neutral benchmarks cannot be met a scheme of mitigation must be submitted which includes on site mitigation that is part of the proposed development and may also include off-site offsetting. The details as approved shall be implemented prior to occupation of the development and thereafter be permanently retained.

Reason: To minimise increased exposure to existing poor air quality and make provision to address local problems of air quality (particularly within AQMAs) (policy 7.14 of the London Plan 2015.)

- 20) Prior to occupation a lighting scheme must be submitted for the approval of the Local Planning Authority in accordance with the Institute of Lighting Professional's Guidance notes for the reduction of obstructive light. The scheme must be designed

by a suitably qualified person in accordance with the recommendations for environmental zone E3 in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:2011.

Before commencement of operation of the approved lighting scheme the applicant shall appoint a suitably qualified member of the institute of lighting professionals (ILP) to validate that the lighting scheme as installed conforms to the recommendations for environmental zone E3 in the ILP document "Guidance Notes for the Reduction of Obtrusive Light GN01:2011

Reason: To ensure minimal nuisance or disturbance is caused to the detriment of the amenities of adjoining occupiers and of the area generally (Policy Q2 (Amenity) – Lambeth Local Plan 2015.)

- 21) The development hereby permitted shall be constructed to include the design principles and provision of physical protection measures to meet Secured by Design, and Secured by Design (New Schools 2014) as related to the uses hereby approved. A certificate of accreditation to Secured by Design standards shall be submitted to the Local Planning Authority for approval in writing prior to first occupation of any part of the development.

Reason: To ensure the safety and security of future occupiers and adjoining properties and prevent crime and disorder occurring within and in the immediate vicinity of the site, in the interest of community safety in accordance with Policies Q2 and Q3 of the Lambeth Local Plan (2015).

- 22) Prior to occupation a Delivery and Servicing Management Plan shall be submitted and approved in writing by the local planning authority. The use hereby permitted shall thereafter be operated in accordance with the approved details. The submitted details must include the following:

- a) frequency of deliveries to the site;
- b) frequency of other servicing vehicles such as refuse collections;
- c) dimensions of delivery and servicing vehicles;
- d) proposed loading and delivery locations including demonstration of their capacity to accommodate the proposed servicing; and
- e) a strategy to manage vehicles servicing the site.

Reason: To avoid obstruction of the surrounding streets and limit the effects of the increase in travel movements within the locality as well as safeguarding public safety and the amenity of the surrounding area. (Policies T6 and T8 of the Lambeth Local Plan).

- 23) The use hereby permitted shall not commence until details and specifications of the proposed cycle parking (including visitor parking) shall be submitted to and approved in writing by the Local Planning Authority. The cycle parking shall be provided in accordance with the approved details before the building hereby permitted is occupied and shall thereafter be retained solely for its designated use.

Reason: To ensure adequate cycle parking is available on site, the design of the cycle parking is acceptable and to promote sustainable modes of transport (Policy T3, Q13 and Q22 of the Lambeth Local Plan).

- 24) Within three months of work starting on site a BREEAM UK New Construction 2014 fully fitted (or such equivalent standard that replaces this) Design Stage certificate and summary score sheet must be submitted to and approved in writing by the Local Planning Authority to show that an Excellent rating will be achieved.

Prior to first occupation of the building(s) a BREEAM UK New Construction 2014 fully fitted (or such equivalent standard that replaces this) Post Construction Review certificate and summary score sheet must be submitted to and approved in writing by the Local Planning Authority to show that a 'Very Good' rating has been achieved. All the measures integrated shall be retained for as long as the development is in existence.

Reason: To ensure that the development has an acceptable level of sustainability (Policy EN4 of the Lambeth Local Plan 2015).

- 25) No non-road mobile machinery (NRMM) shall be used on the application site unless it is compliant with the NRMM Low Emission Zone requirements (or any superseding requirements) and has been registered for the site on the NRMM Register (or any superseding register).

Reason: To ensure that air quality is not adversely affected by the development in line with section 9 of the Local Plan and the Mayor's SPG: The Control of Dust and Emissions during Construction and Demolition.

- 26) No trees other than those identified in the approved Arboricultural Implication (dated 16.05.2017) shall be felled, pruned, uprooted, damaged or otherwise disturbed without the prior written agreement of the Local Planning Authority. All approved tree work shall be carried out in accordance with BS3998:2010.

Reason: To ensure the retention of, and avoid damage to, the retained trees on the site which represent an important visual amenity to the locality (Local Plan Policy Q10).

- 27) The tree protection measures set out in the approved Arboricultural Implication (dated 16.05.2017) shall be put in place before any construction works commence on the site.

Reason: To ensure the retention of, and avoid damage to, the retained trees on the site which represent an important visual amenity to the locality (Local Plan Policy Q10).

- 28) No development above ground level shall commence until of the development details of all hard and soft landscaping (including replacement tree planting) have been submitted to and approved in writing by the Local Planning Authority. The specification shall include details of the quantity, size, species, position and the proposed time of planting of all trees and shrubs to be planted, together with an indication of how they integrate with the proposed in the long term with regard to their mature size and anticipated routine maintenance and protection. In addition all

shrubs and hedges to be planted that are intended to achieve a significant size and presence in the landscape shall be similarly specified.

The landscaping scheme shall include, where practicable, details of measures for rainwater harvesting and surface level water storage. All tree, shrub and hedge planting included within the above specification shall accord with BS3936:1992, BS4043:1989, BS4428:1989, BS8545:2014 and current Arboricultural best practice.

Reasons: In order to introduce high quality landscaping in and around the site in the interests of the ecological value of the site, to contribute to the sustainable management of surface water and to ensure a satisfactory landscaping of the site in the interests of visual amenity (Local Plan Policies Q6, Q9 and Q10).

- 29) The development hereby permitted shall be implemented in accordance with the details set out in the approved Preliminary Ecological Appraisal (dated March 2017)

Reasons: In order to introduce high quality landscaping in and around the site in the interests of the ecological value of the site, to contribute to the sustainable management of surface water and to ensure a satisfactory landscaping of the site in the interests of visual amenity (Local Plan Policies EN1, Q6, Q9 and Q10).

- 30) Prior to first occupation of the school building hereby approved a Community Use Management Plan shall be submitted to and approved in writing by the Local Planning Authority. The occupation of the building shall be in accordance with the approved Community Use Management Plan.

Reason: To ensure that appropriate community access to the school facilities is provided and to protect the amenity of neighbouring properties, to prevent conditions prejudicial to the function and safety of the highway network and to ensure the security of the site in accordance with Policies Q1, Q2, Q3, S2, S3 and T6 of the Lambeth Local Plan (2015).

Informatives(s)

1. This decision letter does not convey an approval or consent which may be required under any enactment, by-law, order or regulation, other than Section 57 of the Town and Country Planning Act 1990.

2. You are advised that this consent is without prejudice to any rights which may be enjoyed by any tenants/occupiers of the premises.

3. Your attention is drawn to the provisions of the Building Regulations, and related legislation which must be complied with to the satisfaction of the Council's Building Control Officer.

4. Your attention is drawn to Sections 4 and 7 of the Chronically Sick and Disabled Persons Act 1970 and the Code of Practice for Access for the Disabled to Buildings (B.S. 5810:1979) regarding the provision of means of access, parking facilities and

sanitary conveniences for the needs of persons visiting, using or employed at the building or premises who are disabled.

5. You are advised of the necessity to consult the Council's Streetcare team within the Public Protection Division with regard to the provision of refuse storage and collection facilities.

6. You are advised of the necessity to consult the Transport and Highways team within the Transport Division of the Directorate of Environmental Services, with regard to any alterations affecting the public footway.

7. Surface Water Drainage - With regard to surface water drainage it is the responsibility of a developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. They can be contacted on 0800 009 3921. Reason - to ensure that the surface water discharge from the site shall not be detrimental to the existing sewerage system.

8. Legal changes under The Water Industry (Scheme for the Adoption of private sewers) Regulations 2011 mean that the sections of pipes you share with your neighbours, or are situated outside of your property boundary which connect to a public sewer are likely to have transferred to Thames Water's ownership. Should your proposed building work fall within 3 metres of these pipes we recommend you contact Thames Water to discuss their status in more detail and to determine if a building over / near to agreement is required. You can contact Thames Water on 0800 009 3921 or for more information please visit our website at www.thameswater.co.uk

9. With reference to Condition 30, relating to the Preliminary Ecological Appraisal, as detailed in Section 6.0 in Table 1, the developer and applicants are hereby advised that the following should be put into practice:

Habitats:

- a) all retained trees should be adequately protected during the work (condition or Tree Protection Plan)
- b) all external landscaping should incorporate native species or those of known biodiversity value.

Birds:

- a) Vegetation clearance should be undertaken outside of the nesting season, and all buildings should be checked for active nesting sites. Any active nests must remain unaffected and further advice be sought from a competent ecological adviser.

- b) Opportunities for house sparrows should be incorporated into the new school building (to be assessed as part any conditions relating to the final designs for the new school buildings, with an expectation that there should be enhancements for birds like house sparrows in any such designs).

Amphibians:

- a) Piles of rubble or other waste materials should be stored in skips, on pallets or to the east of the existing school buildings to minimise the chance of them being used by common amphibians – this can be an advice note for implementation by the applicant.